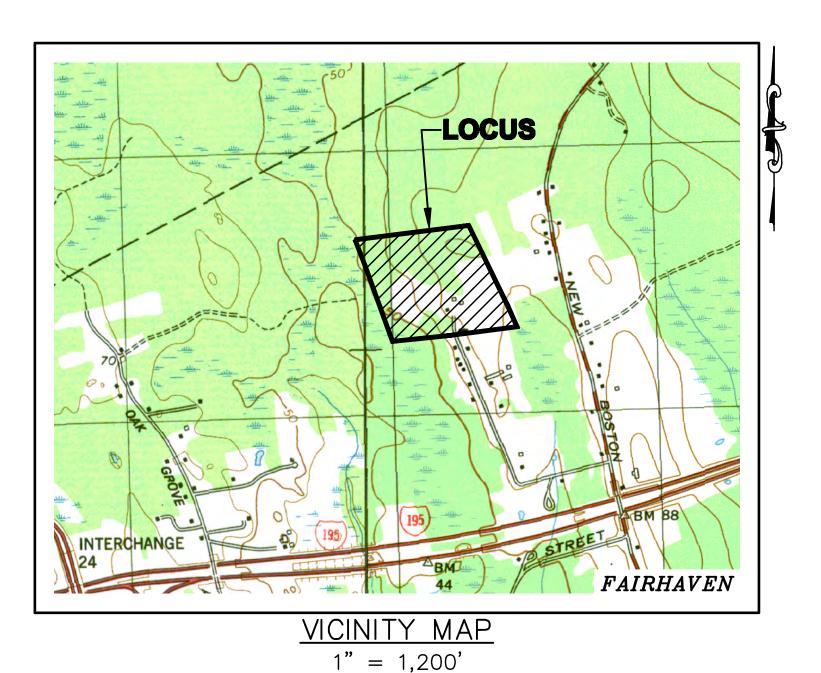
SITE DEVELOPMENT PLANS

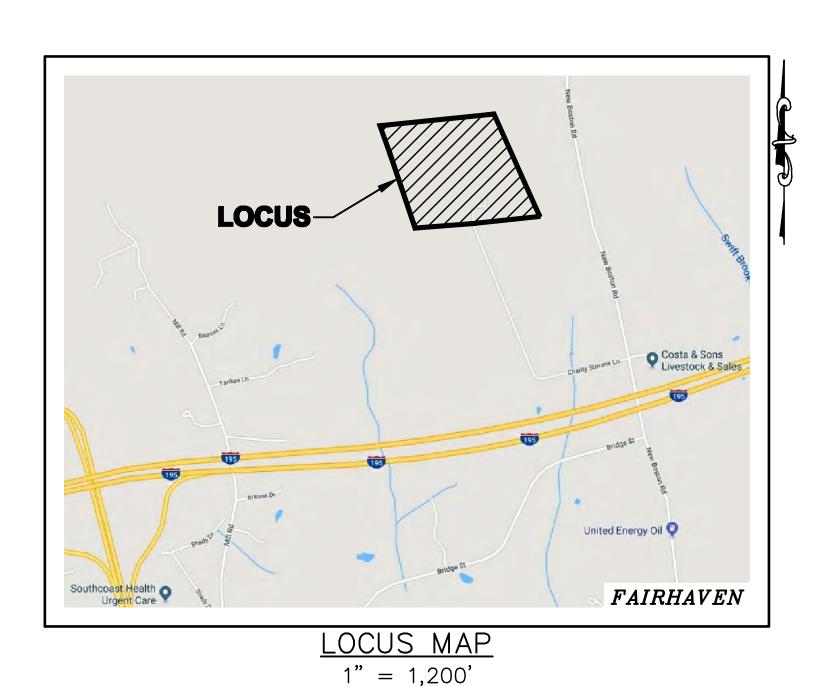
FOR

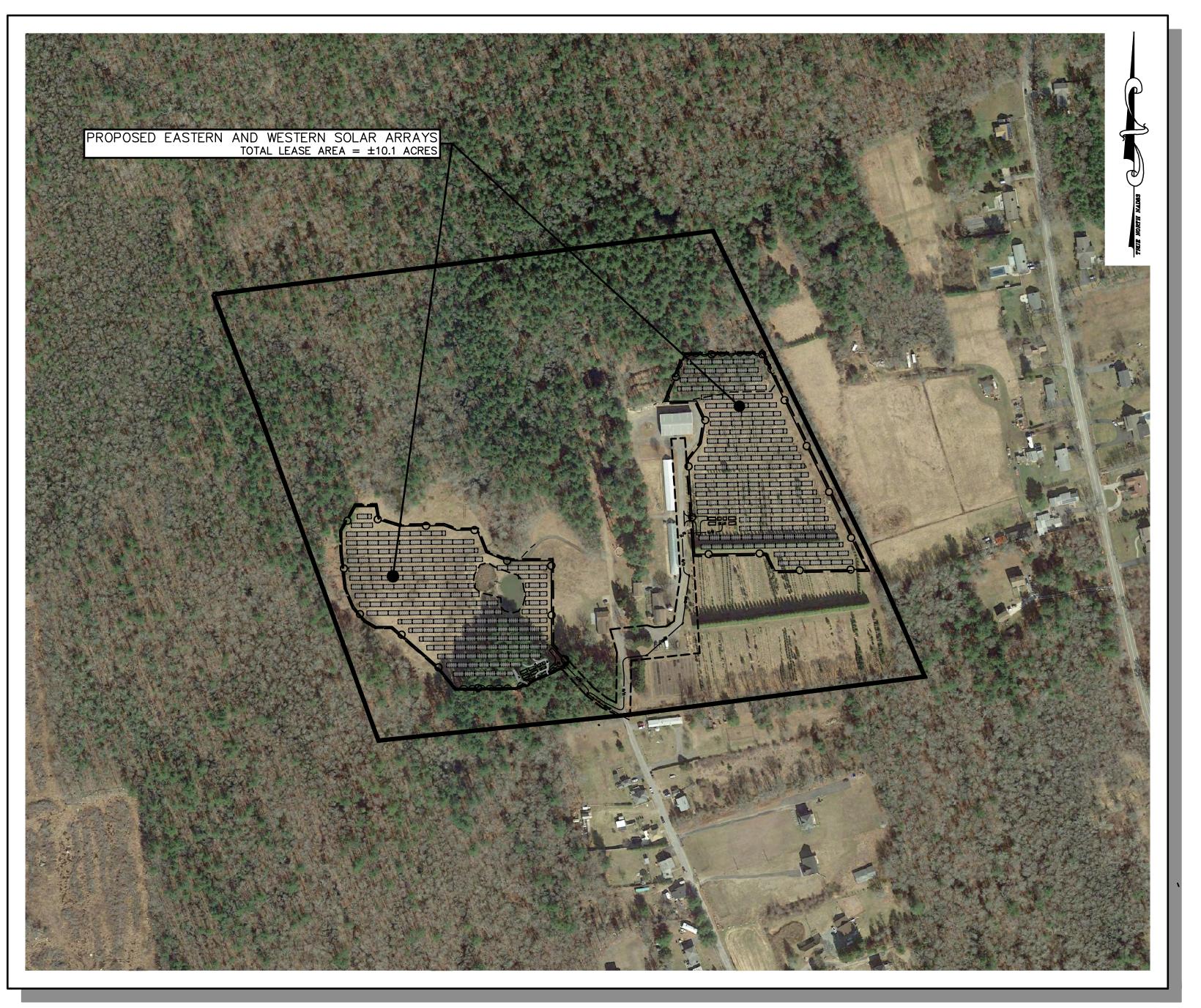
46 CHARITY STEVENS LANE SOLAR PROJECT

FAIRHAVEN, MASSACHUSETTS

DATE: AUGUST 23, 2019
REVISED: SEPTEMBER 11, 2019
REVISED: SEPTEMBER 24, 2019
REVISED: OCTOBER 15, 2019







NOTE:

"THE CONTRACTOR SHALL GIVE SEVEN (7) DAYS'
NOTICE TO PERTINENT TOWN DEPARTMENTS BEFORE
COMMENCING WORK IN THE FIELD"

INDEX OF PLANS		
SHEET NO.	TITLE	SCALE
1 2 3 4 5 6	COVER SHEET OVERALL EXISTING CONDITIONS PLAN OVERALL SITE DEVELOPMENT PLAN SITE DEVELOPMENT PLAN SITE DEVELOPMENT PLAN DETAILS PLAN	1" = 200' 1" = 80' 1" = 80' 1" = 40' 1" = 40' N.T.S.

<u>OWNER:</u>

HASKELL FAMILY REVOCABLE TRUST 46 CHARITY STEVENS LANE FAIRHAVEN, MASSACHUSETTS 02719

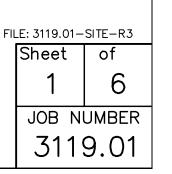
APPLICANT:

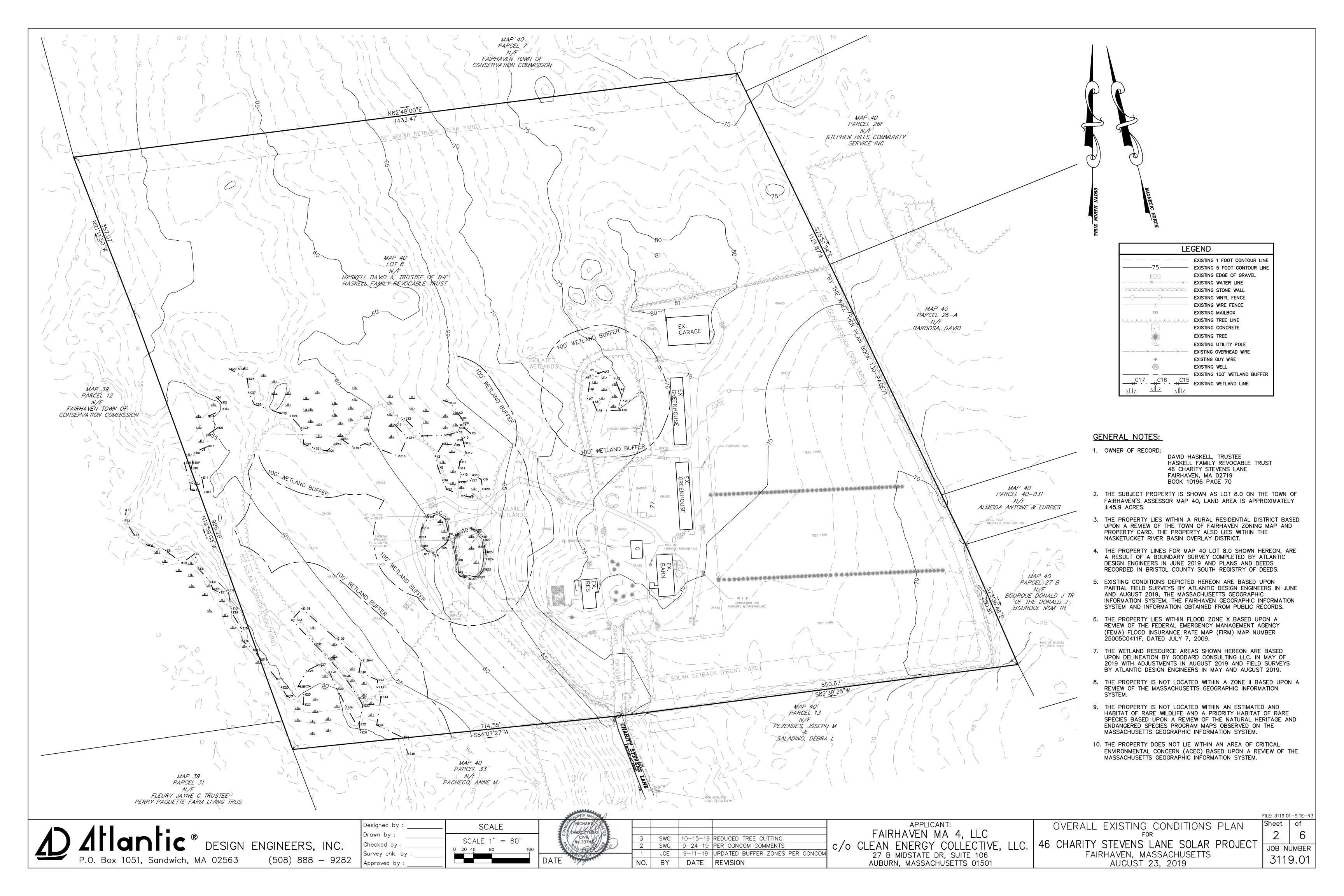
FAIRHAVEN MA 4, LLC C/O CLEAN ENERGY COLLECTIVE, LLC 27 B MIDSTATE DR., SUITE 106 AUBURN, MASSACHUSETTS 01501

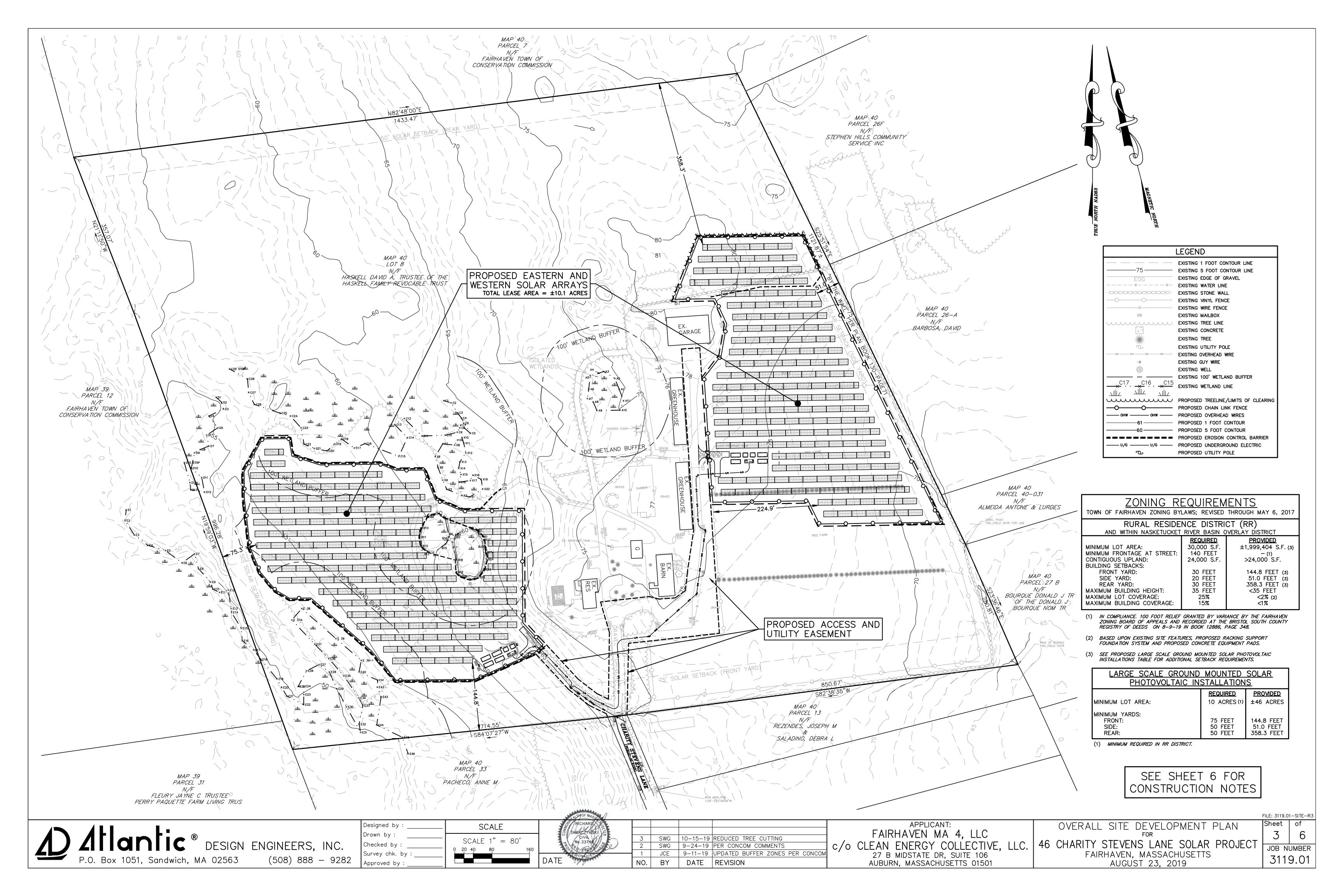
ENGINEER:

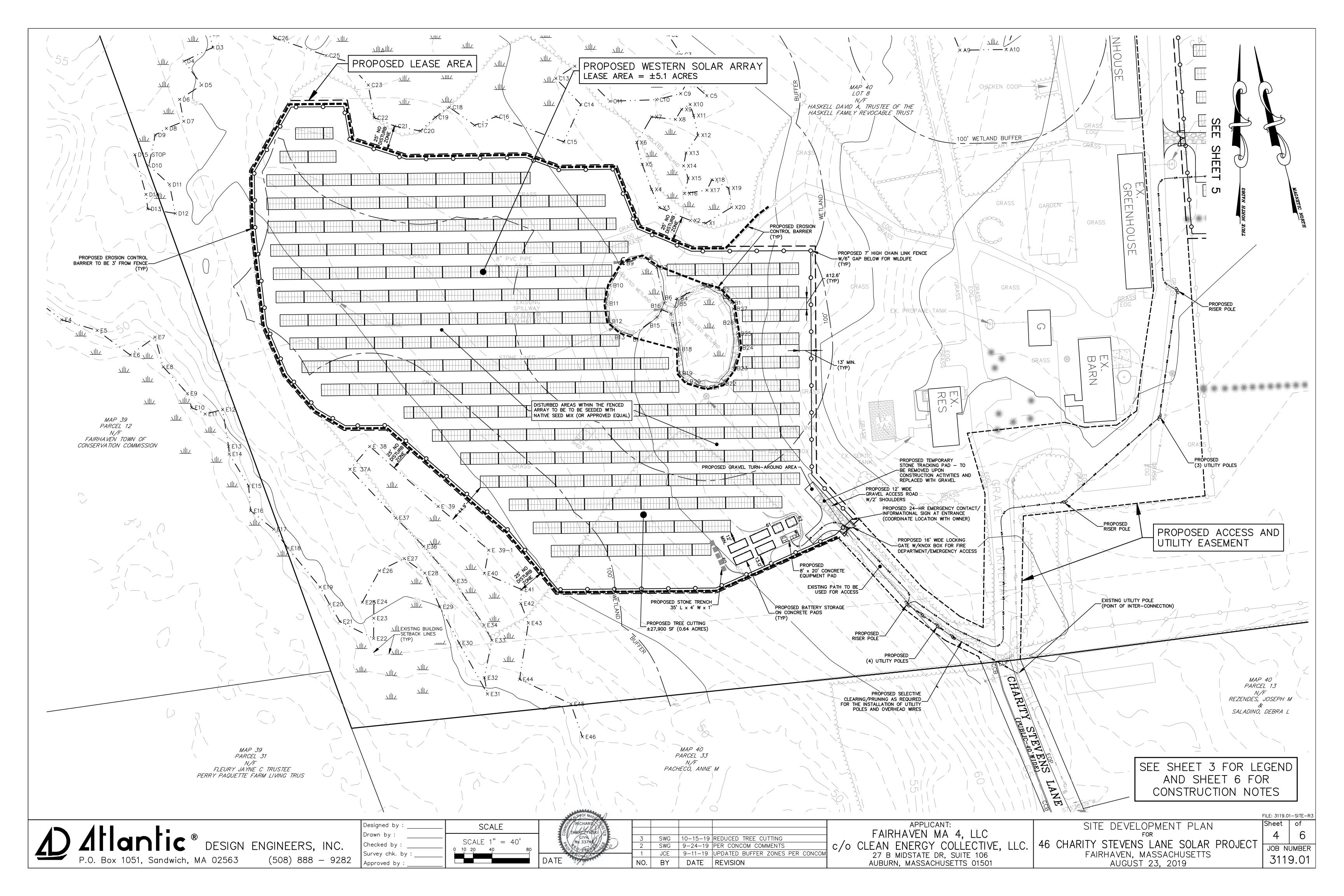


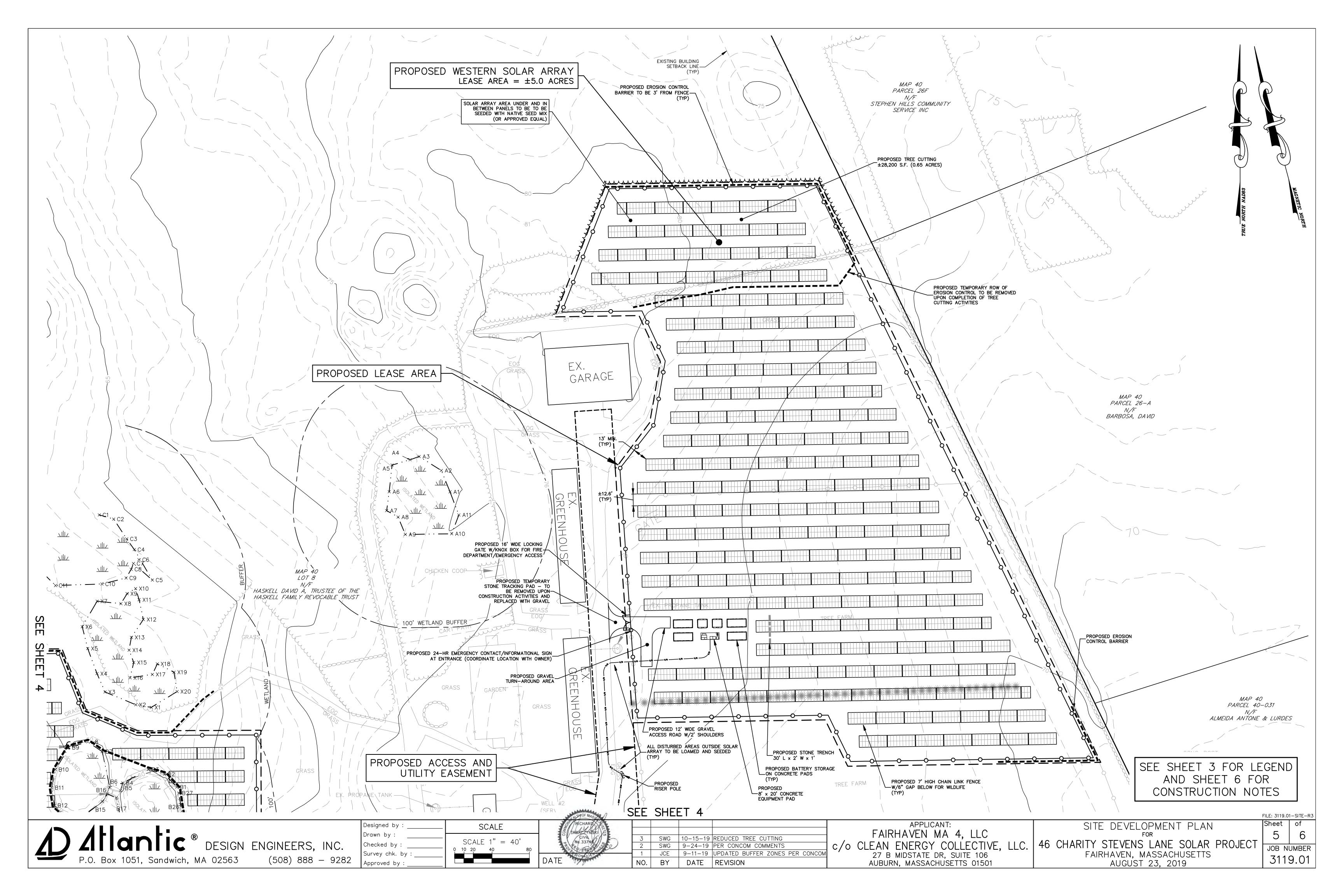












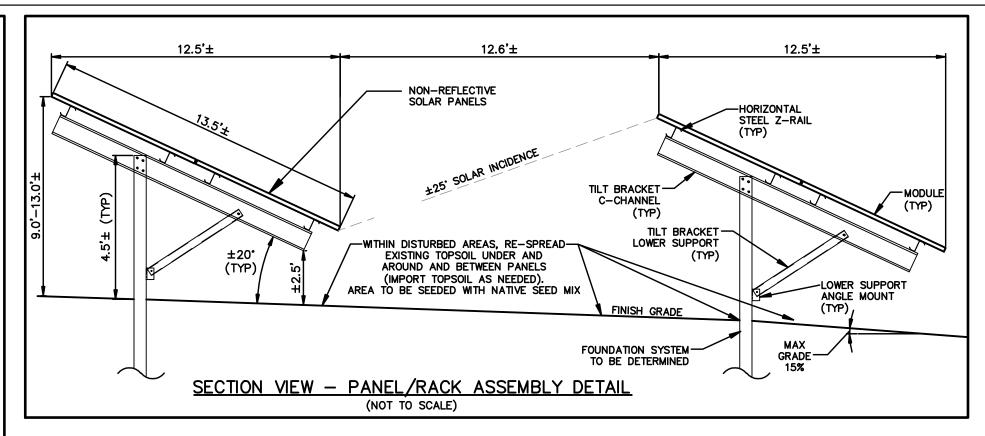


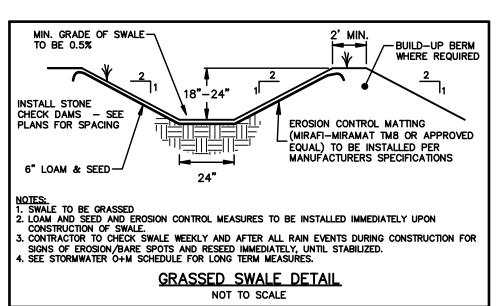
46 Charity Stevens Lane Fairhaven, MA 02719

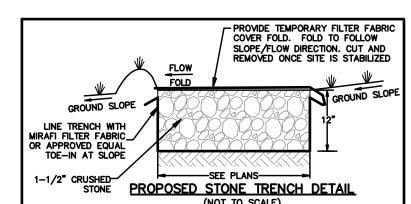
24-Hour Emergency Contact #: 844-232-7253

CleanEnergyCo.com

NOTE - SIGNAGE TO CONFORM TO LOCAL SIGNAGE REQUIREMENTS. FINAL SIGN DESIGN TO BE SUBMITTED AS PART OF BUILDING PERMIT APPLICATION PACKAGE. TYPICAL SITE INFORMATION/24-HR EMERGENCY CONTACT SIGN (NOT TO SCALE)







SEDIMENT AND EROSION CONTROL NOTES:

- PRIOR TO COMMENCING SITE WORK OR EARTHWORK OPERATIONS, INSTALL SEDIMENT CONTROL BARRIERS AND MAINTAIN THROUGHOUT CONSTRUCTION.
- 2. ALL DISTURBED AREAS SHALL BE LOAMED AND SEEDED IMMEDIATELY UPON COMPLETION OF CONSTRUCTION.
- ALL MATERIALS AND STOCKPILES SHALL BE STORED ON LEVEL AREAS OUTSIDE OF ANY FLOOD ZONES, WETLANDS OR BUFFER ZONE AREAS. ALL STOCKPILES SHALL BE SURROUNDED BY SILT SOCK, SHALL HAVE SIDE SLOPES NO GREATER THAN 30% AND SHALL BE SEEDED OR STABILIZED IF
- SEDIMENTATION CONTROL DEVICES AND EROSION CONTROL BARRIERS SHALL BE INSPECTED WEEKLY AND MAINTAINED AS NECESSARY THROUGHOUT ALL PHASES OF CONSTRUCTION AND PROMPTLY AFTER EACH
- 5. ANY SLOPE STEEPER THAN 3:1 SHALL BE EQUIPPED WITH SLOPE STABILIZATION FABRIC OR EROSION CONTROL MATTING.
- 6. ADDITIONAL SEDIMENT CONTROL MEASURES SHALL BE INSTITUTED AS CONDITIONS WARRANT OR AS DIRECTED BY THE ENGINEER AND/OR THE
- THE CONTRACTOR MUST REPAIR OR RE-SEED ANY AREAS THAT DO NOT DEVELOP WITHIN A PERIOD OF ONE YEAR AT NO ADDITIONAL EXPENSE TO
- 8. MATERIAL STOCKPILES SHALL NOT BE LOCATED WITHIN THE PATH OF EXISTING OR PROPOSED WATERCOURSES (BOTH TEMPORARY OR
- PERMANENT) OR THOSE AREAS SUBJECT TO STORM WATER FLOW. SEDIMENT CONTROL DEVICES AND SEDIMENT CONTROL BARRIERS MAY BE REMOVED ONLY AFTER THE SITE HAS BEEN STABILIZED AND APPROVED BY
- 10. ALL DISTURBED OR EXPOSED AREAS SUBJECT TO EROSION, WHICH REMAIN DISTURBED BUT INACTIVE FOR AT LEAST 14 DAYS, SHALL RECEIVE TEMPORARY SEEDING IN ACCORDANCE WITH THE MASSACHUSETTS EROSION AND SEDIMENT CONTROL GUIDELINES. IN ALL CASES, STABILIZATION MEASURES SHALL BE IMPLEMENTED AS SOON AS POSSIBLE IN ACCORDANCE WITH THE MASSACHUSETTS EROSION AND SEDIMENT CONTROL GUIDELINES.
- 11. EARTHWORK ACTIVITY ON THE SITE SHALL BE DONE IN A MANNER SUCH THAT RUNOFF IS DIRECTED AWAY FROM ABUTTING STRUCTURES,
- 12. THE CONTRACTOR SHALL KEEP ON SITE AT ALL TIMES ADDITIONAL SILT SOCK AND EXTRA SILTATION FENCING FOR INSTALLATION AT THE DIRECTION OF THE ENGINEERS, CONSERVATION COMMISSION, OR THE BOARD TO MITIGATE ANY EMERGENCY CONTROL.
- 13. REFER TO CONSTRUCTION DETAILS FOR ADDITIONAL EROSION CONTROL
- 14. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE SITING, RELOCATION AND AUGMENTATION OF SEDIMENT AND EROSION CONTROL DEVICES AS THE PROJECT PROGRESSES AND THE SITE DRAINAGE CONDITIONS CHANGE.
- 15. THE CONTRACTOR SHALL MINIMIZE THE AREA OF DISTURBED SOIL.

 EFFORTS SHALL BE MADE TO LIMIT THE TIME OF EXPOSURE OF DISTURBED
- 16. ANY DISTURBED SOILS NOT PERMANENTLY STABILIZED PRIOR TO OCTOBER 15 OF ANY YEAR SHALL BE TEMPORARILY STABILIZED TO PREVENT EROSION UNTIL ACTIVE USE RESUMES..

CONSTRUCTION PERIOD STORMWATER OPERATION AND MAINTENANCE:

THE CONSTRUCTION ENTRANCE TRACKING PADS SHOULD BE MAINTAINED IN A CONDITION THAT WILL PREVENT TRACKING OR FLOW OF SEDIMENT ONTO THE

ADDITIONAL STONE. THE ENTRANCE PAD SHOULD BE INSPECTED WEEKLY AT A MINIMUM, AFTER MAJOR STORM EVENTS (2" OR GREATER) AND DURING

PUBLIC RIGHT-OF-WAY. THIS MAY REQUIRE PERIODIC TOP DRESSING WITH

THE GRAVEL, THE PAD SHOULD BE TOP DRESSED WITH NEW STONE OR REPLACED COMPLETELY. WHERE SEDIMENT HAS BEEN TRACKED—OUT FROM YOUR SITE ONTO PAVED ROADS, SIDEWALKS, OR OTHER PAVED AREAS OUTSIDE OF YOUR SITE, REMOVE THE DEPOSITED SEDIMENT BY THE END OF

OF THE NEXT BUSINESS DAY IF TRACK-OUT OCCURS ON A NON-BUSINESS

TRACKED-OUT SEDIMENT INTO ANY STORMWATER CONVEYANCE, STORM DRAIN

LEAST DAILY DURING PROLONGED RAINFALL. AFTER CONSTRUCTION, INSPECT AT

ADEQUATELY. ACCUMULATED SEDIMENT SHALL BE REMOVED AT LEAST ONCE A

DAY, REMOVE THE TRACK-OUT BY SWEEPING, SHOVELING, OR VACUUMING

THESE SURFACES, OR BY USING OTHER SIMILARLY EFFECTIVE MEANS OF SEDIMENT REMOVAL. YOU ARE PROHIBITED FROM HOSING OR SWEEPING

DURING CONSTRUCTION GRASSED LINED SWALES SHALL BE INSPECTED IMMEDIATELY AFTER EACH RUN-OFF PRODUCING RAINFALL EVENT AND AT

A MINIMUM OF TWICE A YEAR, OR AFTER MAJOR STORM EVENTS (2" OR

ADDITIONAL INSPECTIONS SHOULD BE SCHEDULED DURING THE FIRST FEW MONTHS TO ENSURE THAT THE VEGETATION IN THE CHANNELS IS ESTABLISHED

YEAR OR BEFORE IT EXCEEDS 0.5' IN DEPTH, WHICHEVER OCCURS FIRST. SWALES SHALL BE MOWED AS NEEDED. CLIPPINGS TO BE REMOVED FROM SWALES, AREAS IMMEDIATELY UP-GRADIENT AND PROPERLY DISPOSED OF.

INSPECT AFTER EVERY MAJOR STORM EVENT (2" OR GREATER) DURING CONSTRUCTION. ONCE SITE IS STABILIZED AND RE-VEGETATED, CUT

AWAY/REMOVE TEMPORARY COVER FOLD AND INSPECT AND FOR THE FIRST

FEW MONTHS AFTER CONSTRUCTION TO ENSURE PROPER STABILIZATION AND

WEATHER TO ENSURE THE SYSTEM IS WORKING PROPERLY. REMOVE SEDIMENT

AS NECESSARY DURING CONSTRUCTION, AND AT LEAST EVERY FIVE YEARS

THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROPER INSPECTION AND

UPON COMPLETION OF CONSTRUCTION, THE OPERATION AND MAINTENANCE OF ALL COMPONENTS OF THE STORMWATER MANAGEMENT SYSTEM WILL BE THE

> c/o CLEAN ENERGY COLLECTIVE, LLC. 27 B MIDSTATE DR, SUITE 106

AUBURN, MASSACHUSETTS 01501

ACCORDANCE WITH APPLICABLE LOCAL, STATE AND FEDERAL GUIDELINES AND

FAIRHAVEN MA 4, LLC

3. DISPOSAL OF ACCUMULATED SEDIMENT AND HYDROCARBONS TO BE IN

4. THERE SHALL BE NO ILLICIT DISCHARGE OF ANY WASTE OR WASTE WATER

INTO THE STORMWATER MANAGEMENT SYSTEM. THE MAINTENANCE OF THE

RESPONSIBILITY OF THE SYSTEM OWNER:

FUNCTION. THEREAFTER INSPECT AT LEAST TWICE PER YEAR DURING WET

AFTER CONSTRUCTION. ONCE CONSTRUCTION IS COMPLETED CHECK FOR ACCUMULATION OF SEDIMENT, DEBRIS AND LEAF LITTER TWICE A YEAR

GREATER). REPAIR ERODED SPOTS IMMEDIATELY AFTER INSPECTION.

SCHEDULE:

SEDIMENT CONTROL BARRIERS:

INLET, OR WATER OF THE U.S.*)

STONE TRACKING PAD:

GRASSED SWALE:

STONE TRENCH:

1. THE PROPERTY LINES FOR MAP 40 LOT 8.0 SHOWN HEREON, ARE A RESULT OF A BOUNDARY SURVEY COMPLETED BY ATLANTIC DESIGN ENGINEERS IN JUNE 2019 AND PLANS AND DEEDS RECORDED IN BRISTOL SEDIMENT CONTROL BARRIERS (SILT SOCK, ETC.) SHOULD BE INSPECTED

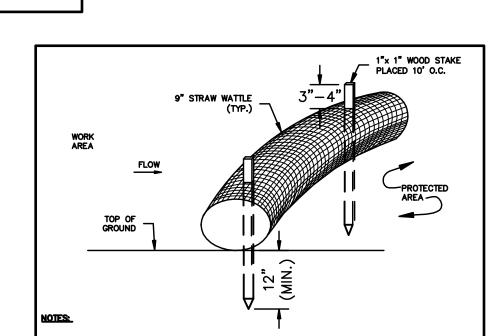
CONSTRUCTION NOTES:

PUBLIC RECORDS.

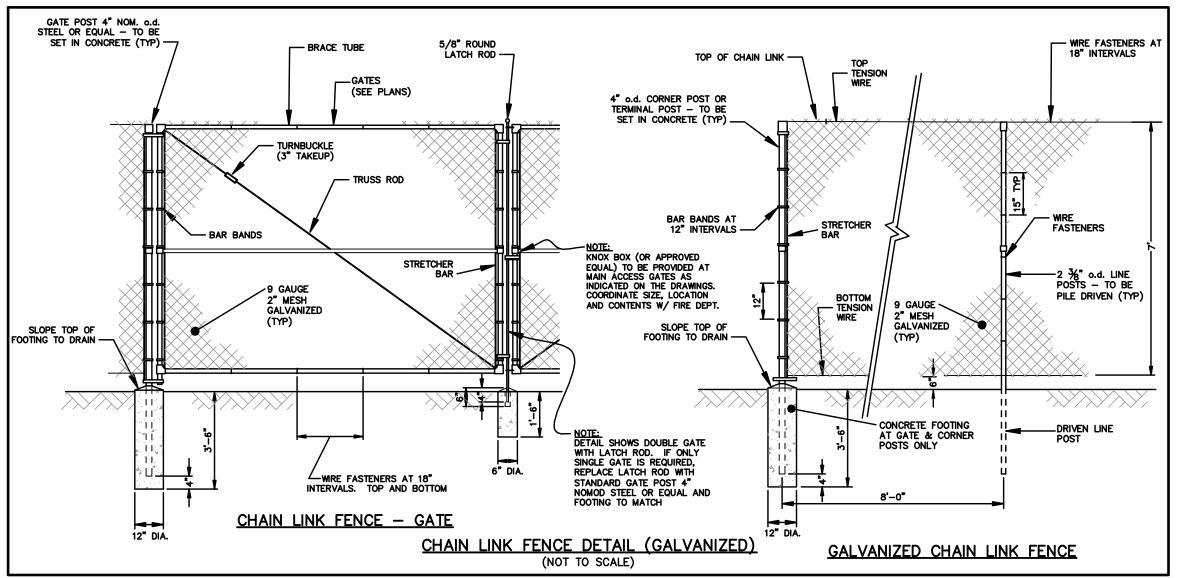
- IMMEDIATELY AFTER EACH RUN-OFF PRODUCING RAINFALL EVENT AND AT EXISTING CONDITIONS DEPICTED HEREON ARE BASED UPON A PARTIAL LEAST DAILY DURING PROLONGED RAINFALL. SEDIMENT DEPOSITS MUST BE REMOVED WHEN THE LEVEL OF DEPOSITION REACHES APPROXIMATELY ONE—HALF THE HEIGHT OF THE BARRIER. SEDIMENT SHOULD BE DISPOSED OF IN A SUITABLE AREA AND PROTECTED FROM EROSION BY EITHER STRUCTURAL FIELD SURVEY BY ATLANTIC DESIGN ENGINEERS IN JUNE 2019, THE MASSACHUSETTS GEOGRAPHIC INFORMATION SYSTEM, THE FAIRHAVEN GEOGRAPHIC INFORMATION SYSTEM AND INFORMATION OBTAINED FROM
 - 3. THE CONTRACTOR SHALL VERIFY ALL EXISTING CONDITIONS SHOWN AND NOTIFY THE ENGINEER OF ANY DISCREPANCIES THAT MAY BE FOUND IN
 - CONTRACTOR SHALL VERIFY ALL CRITICAL ELEVATIONS AND INVERTS PRIOR
- PERIODS OF HEAVY USE. WHEN MUD AND SOIL PARTICLES CLOG THE VOIDS II WHERE AN EXISTING PUBLIC UTILITY IS FOUND TO CONFLICT WITH THE PROPOSED WORK, THE LOCATION, ELEVATION AND SIZE OF THE UTILITY SHALL BE ACCURATELY DETERMINED AND THE INFORMATION FURNISHED TO THE UTILITY COMPANY AND OWNER FOR RESOLUTION OF THE CONFLICT THE SAME BUSINESS DAY IN WHICH THE TRACK-OUT OCCURS OR BY THE END
 - CONSIDERED AS PART OF THIS SURVEY. NO STATEMENT IS MADE
 CONCERNING THE EXISTENCE OF UNDERGROUND OR OVERHEAD
 CONTAINERS OR FACILITIES THAT MAY AFFECT THE USE OR DEVELOPMENT 7. IT IS THE CONTRACTOR'S RESPONSIBILITY TO NOTIFY DIGSAFE, THE TOWN OF FAIRHAVEN DEPARTMENT OF PUBLIC WORKS AND ALL UTILITY COMPANIES A MINIMUM OF 72 HOURS PRIOR TO CONSTRUCTION ACTIVITIES FOR LOCATION OF ALL UNDERGROUND UTILITIES AND UTILITY
 - 8. ALL BUILDINGS, SURFACE, AND SUBSURFACE IMPROVEMENTS ON AREAS ADJACENT TO THE SITE ARE NOT NECESSARILY SHOWN HEREON.
 - 9. THE LOCATION OF EXISTING UNDERGROUND UTILITIES ARE SHOWN IN AN APPROXIMATE WAY AND HAVE NOT BEEN INDEPENDENTLY VERIFIED BY THE OWNER OR ITS REPRESENTATIVES. THE CONTRACTOR SHALL DETERMINE THE EXACT LOCATION OF ALL UTILITIES AND RIM AND INVERTS BEFORE COMMENCING WORK. AND AGREES TO BE FULLY RESPONSIBLE FOR ANY AND ALL DAMAGES THAT MIGHT OCCUR BY THE CONTRACTOR'S FAILURE TO EXACTLY LOCATE AND PRESERVE ANY AND ALL UNDERGROUND
 - 10. THE CONTRACTOR SHALL MAKE ALL ARRANGEMENTS FOR THE ALTERATION AND ADJUSTMENTS OF ELECTRIC, TELEPHONE AND ANY OTHER PRIVATE UTILITIES WITH THE UTILITY COMPANY, IF NECESSARY, IF ANY INTERRUPTIONS IN SERVICE ARE NECESSARY TO ABUTTING PROPERTY OWNERS, A MINIMUM OF 48 HOURS NOTICE SHALL BE GIVEN.
 - 11. THE CONTRACTOR SHALL PROVIDE TRAFFIC CONTROL MEASURES IN ACCORDANCE WITH THE MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES AND MASSACHUSETTS HIGHWAY DEPARTMENT REQUIREMENTS FOR ALL WORK WITHIN PUBLIC STREETS.
 - 12. AREAS OUTSIDE THE LIMITS OF PROPOSED WORK DISTURBED BY THE CONTRACTOR'S OPERATION SHALL BE RESTORED BY THE CONTRACTOR TO
- MAINTENANCE OF ALL STORMWATER AND EROSION CONTROL FACILITIES UNTIL THEIR ORIGINAL CONDITION AT THE CONTRACTOR'S EXPENSE. THE PROJECT CONSTRUCTION IS COMPLETED. THE CONTRACTOR SHALL CLEAN ALL COMPONENTS OF THE STORMWATER MANAGEMENT SYSTEM AT THE COMPLETION OF CONSTRUCTION, IMMEDIATELY PRIOR TO TURNING OVER OPERATION AND MAINTENANCE RESPONSIBILITY TO THE PROJECT PROPONENT. 13. THE CONTRACTOR IS RESPONSIBLE TO ENSURE THAT ALL REQUIRED INSPECTIONS AND/OR CERTIFICATIONS REQUIRED BY CODES AND/OR UTILITY COMPANIES ARE COMPLETED PRIOR TO INSTALLATION, BACKFILLING, ANNOUNCED BUILDING POSSESSION, AND THE FINAL CONNECTION OF
 - 14. THE CONTRACTOR SHALL BE RESPONSIBLE FOR MAINTAINING ALL SURVEY CONTROL POINTS AND BENCHMARKS NECESSARY FOR THE PROPOSED
 - 15. THE CONTRACTOR SHALL BE RESPONSIBLE FOR MAINTAINING ADEQUATE
- RECORDS OF THE LOCATION AND ELEVATION OF ALL WORK INSTALLED. 16. THE CONTRACTOR SHALL INSTITUTE AND MAINTAIN ALL SAFETY MEASURES NECESSARY TO PROTECT THE PUBLIC DURING CONSTRUCTION, INCLUDING, BUT NOT LIMITED TO BARRICADES, SIGNS, FENCES, FLAGGERS, LIGHTING, POLICE DETAIL, AND ANY OTHER MEANS AS DIRECTED BY THE TOWN. NO THE SYSTEM OWNER SHALL COMPLY WITH THE POST CONSTRUCTION LONG TERM STORMWATER OPERATION AND MAINTENANCE PLAN APPROVED FOR THIS PROJECT.

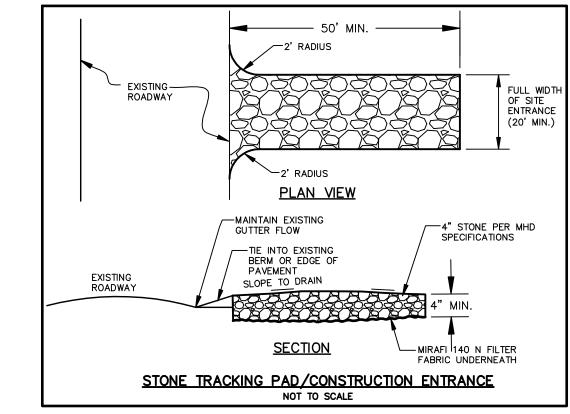
TRENCHES ARE TO REMAIN OPEN OVERNIGHT.

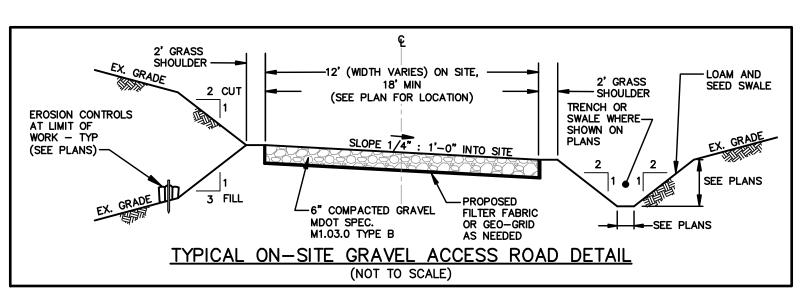
- 17. THE CONTRACTOR SHALL KEEP THE PREMISES FREE FROM THE ACCUMULATION OF WASTE MATERIAL AND OTHER DEBRIS RESULTING FROM THE WORK. AT THE END OF CONSTRUCTION THE CONTRACTOR SHALL REMOVE ALL CONSTRUCTION DEBRIS AND SURPLUS MATERIALS FROM THE SITE. A THOROUGH INSPECTION OF THE WORK PERIMETER IS TO BE MADE FACILITY SHALL BE UNDERTAKEN IN SUCH A MANNER AS TO PREVENT ANY DISCHARGE OF WASTE OR WASTE WATER INTO STORMWATER MANAGEMENT SYSTEM. ANY WASTE OIL OR OTHER WASTE PRODUCTS GENERATED DURING MAINTENANCE SHALL BE PROPERLY DISPOSED OF OFF SITE. AND ALL DISCARDED MATERIALS, BLOWN OR WATER CARRIED DEBRIS, SHALL BE COLLECTED AND REMOVED FROM THE SITE.
 - 18. ALL WORK SHALL BE DONE IN STRICT COMPLIANCE WITH ALL APPROVED PERMITS AND WITH ALL APPLICABLE NATIONAL, STATE, AND LOCAL CODES,
 - 19. CONTRACTOR TO DESIGNATE A SPECIFIC AREA FOR COMBUSTIBLE MATERIALS, APPROVED BY THE FIRE DEPARTMENT, SO THAT COMBUSTIBLES ARE NOT SPREAD THROUGHOUT THE CONSTRUCTION SITE.
 - STORMWATER POLLUTION PREVENTION PLAN (SWPPP) FOR CONSTRUCTION ACTIVITIES FOR THIS PROJECT.

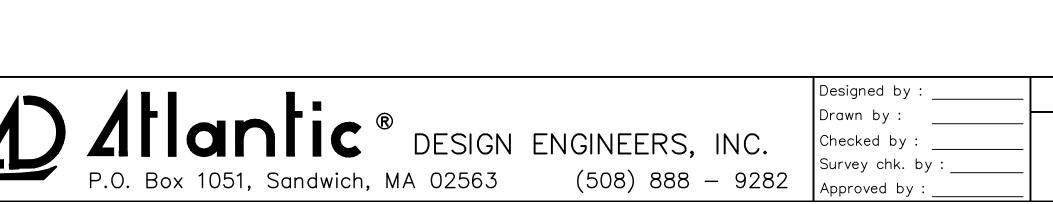


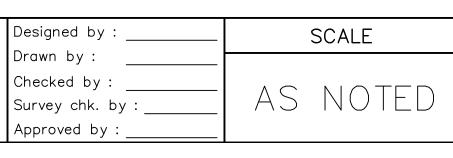
STRAW WATTLE — EROSION CONTROL DETAIL (NOT TO SCALE)

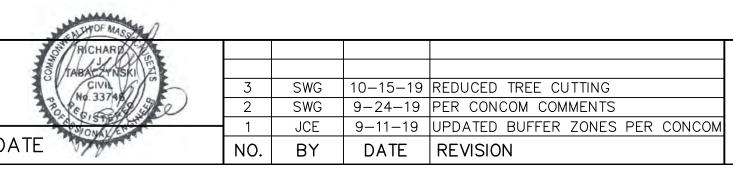












DETAILS PLAN